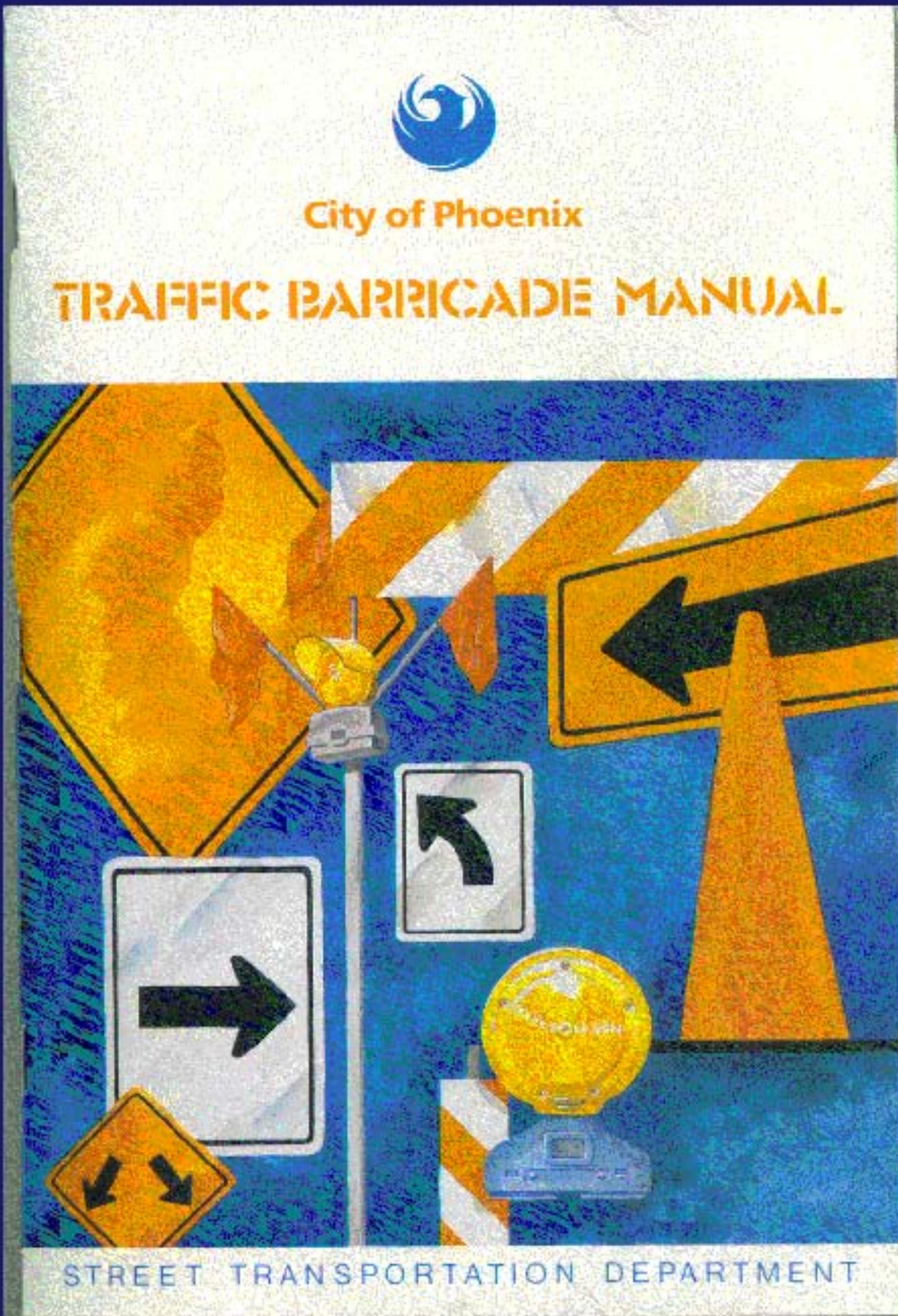


The link below was valid on June 1st, 2006 and allowed the web visitor to view the [City Of Phoenix Traffic Barricade Manual](http://www.ci.phoenix.az.us/AGENCY/phxutper/b1.html) ( and addendums ) on line.



Please visit [Phoenix At Your Fingertips](#) for citizen and public information.

[\\*Begin View of Traffic Barricade Manual](#)



# EVALUATION GUIDE WORK ZONE SIGNS

## Acceptable

There are several abrasions on the surface but very little loss of lettering. There has been no touch-up of the lettering. This message is legible per the design criteria of the MUTCD.



## Marginal

Of the many surface abrasions throughout the sign face, many are within the individual letters of the message. The sign surface is free of any residue. Although some color fading is evident, the background color and reflectivity are still apparent at night. This message is legible per the design criteria of the MUTCD.



## Unacceptable

Signs with asphalt splatter or cement slurry of an amount similar to the abrasions that are evident throughout the face of this sign are unacceptable. Some letters have a loss of more than 50 percent. There is a noticeable color fading. The message is illegible per the design criteria of the MUTCD.



# EVALUATION GUIDE TYPE I, II or III BARRICADE PANELS OR VERTICAL PANELS

## Acceptable

Panels not deformed to an extent so as to decrease the panels target value. There are several abrasions on the surface but very little loss of reflective sheeting. The orange is vivid and the stripes provide contrast.



## Marginal

There are numerous surface abrasions through the panel surface. Some color fading is evident; however, it is free of large areas of residue or missing reflective material. The orange is vivid and the stripes provide contrast.



## Unacceptable

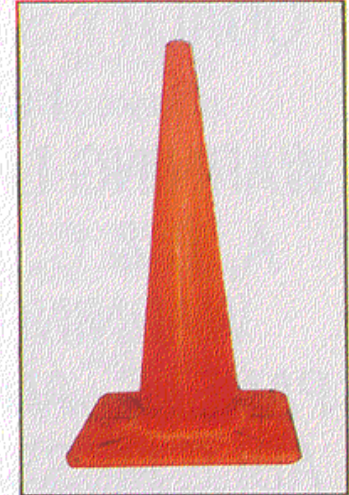
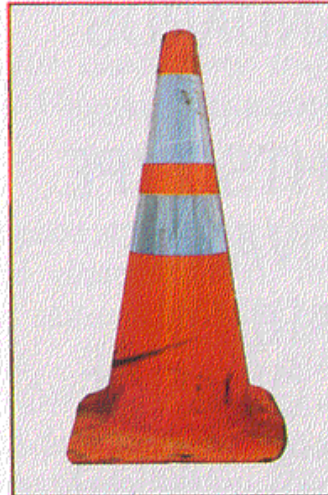
The surface is marred over a high percentage of the panel area. There is noticeable loss of reflectivity and obvious color fading. Panels with asphalt splatter and/or cement slurry, or any combination of missing and covered reflective material similar in area to that shown here would also make a panel unacceptable.



# EVALUATION GUIDE CONES

## Acceptable

The conical shape should remain clearly identifiable with no significant distortion and must be free standing in its normal position. The surface is free of punctures and abrasions. The surface is free of asphalt splatter, cement slurry or other material and will readily respond to washing. The reflective bands, if required, have little or no loss of reflectivity, with only minor tears and scratches.



## Marginal

The surface has some asphalt splatterings or cement slurry and may not be readily cleaned due to abrasion and discoloration. The reflective bands, if required, have numerous tears and scratches, but are free of large areas of residue or missing material.



## Unacceptable

Punctures and large areas of staining asphalt splatter or cement slurry make these an unlikely candidate for improvement. Large areas of missing or stained reflective material make the cone unacceptable.



# QUALITY STANDARDS FOR WARNING LIGHTS TYPE A, B & C, ADVANCE WARNING ARROW PANELS, AND CHANGE- ABLE MESSAGE SIGNS

This standard applies to all Type A, B & C warning lights, advance warning arrow panels, and changeable message signs that are furnished by the agency, supplier, subcontractor, or contractor and used for traffic control in work zones.

The use and placement of Type A, B & C warning lights, advance warning arrow panels, and changeable message signs are specified in the contract documents. All Type A, B & C warning lights, advance warning arrow panels, and changeable message signs shall be in accordance with the most current version of the Manual on Uniform Traffic Control Devices (MUTCD).

For Type A, B & C warning lights to be functioning properly, they must meet the MUTCD criteria which states: "Type A low intensity flashing warning lights and Type C steady burn warning lights shall be maintained so as to be capable of being visible on a clear night from a distance of 3,000 feet. Type B high intensity flashing warning lights shall be maintained so as to be capable of being visible on a sunny

day when viewed without the sun directly on or behind the device from a distance of 1,000 feet".

The evaluation guide that follows is to be used to evaluate the appearance and function of Type A, B & C warning lights, advance warning arrow panels, and changeable message signs. Because of the different types of advance warning arrow panels approved for use, the evaluation guide will address each type (mode) of panel separately.

Any warning light, arrow panel, or changeable message sign which is out of alignment from the intended driver's line of vision, shall be considered to be "unacceptable".

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## City of Phoenix

October 25, 1999

Dear City of Phoenix Traffic Barricade Manual User:

### RE: STORAGE AND REMOVAL OF TEMPORARY TRAFFIC CONTROL DEVICES

The City of Phoenix has long been a leader in work zone traffic control, due to a strong partnership between private enterprise and the City. We want to keep it that way, and recently we have noted serious erosion of care and caution regarding storage of barricades and removal of temporary traffic control devices. In order to share our expectations with the industry, regarding the storage and removal of temporary traffic control devices, we have prepared a formal addendum to the 1998 Traffic Barricade Manual (TBM).

In recent months, complaints have been received from neighborhoods, businesses, and elected officials (aesthetics), from the Mayor's Commission on Disabilities (devices left in such a way as to make sidewalks impassable), and from motorists encountering non-applicable traffic control. When checked out, these incidents occurred when contractors or barricade companies failed to properly store and/or remove temporary traffic control; or improperly left devices causing obstructions to pedestrian, bicyclist, and vehicular traffic accessibility. Worse yet, we are finding barricades/vertical panels/warning signs alongside the road for extended periods of time when NO construction and/or maintenance work is taking place. This is incorrect, as the public right-of-way cannot be used simply to warehouse temporary traffic control.

Our last TBM ALERT dealt with keeping sidewalks accessible and the proper placement, storage, and removal of temporary traffic control devices when not in active use in the public right-of-way. While we have seen some improvement, progress remains unsatisfactory. Placement of inactive temporary traffic control is every bit as important as placement of the same devices being placed for actual work zone traffic control. Consideration must be given to all affected users of the public right-of-way at all times. Maintaining clear sidewalk access to bus stops, local merchants, facilities, and crosswalks must be an integral part of each project undertaken. The provisions for public protection established in the TBM are applicable to ALL persons, contractors, utilities, and other agencies including City forces doing work within the City.

The attached addendum to the TBM is being distributed to all contractors, utility companies, barricade companies, and City departments who have personnel working in the public right-of-way. By ordinance, each barricade/traffic control device in the street is identified with the name of the vendor/phone numbers, so the owner may be contacted. When emergency conditions occur, immediate action must be taken to protect the public.

In summary, it is essential that accessibility to facilities be maintained for all users of the public right-of-way. This is essential for public relations and needs to be a heartfelt goal of each and every person involved in street construction and street management. Both industry and the City need to do what is necessary to improve the situation. Because of the problems, suggestions have been made for the City to:

- > Establish a system of financial sanctions for non-compliance; or
- > Establish a system whereby a fee is charged for each day traffic control is left on right-of-way; or
- > Treat improperly stored traffic control similar to illegally parked vehicles with tow-away rights, storage charges, and charges for barricades to be released.

The City does not want to have to establish such rigorous/punitive procedures, unless we have to. Our system has worked before and it can work again, but **ONLY** if the industry through American Traffic Safety Services Association takes a leadership role. The City is requesting **strict** compliance/enforcement of this policy.

Handwritten signature of James W. Sparks in black ink.

James W. Sparks, P.E.  
Deputy Street Transportation Director

Handwritten signature of Anthony P. Arviso, Sr. in black ink.

Anthony P. Arviso, Sr.  
Construction Traffic Control Supervisor

AVS:MSF:APAS:CAALERT99.DCC

Attachment

# **Storage and Removal of Temporary Traffic Control Devices in Phoenix Right-of-Way**

(Official Addendum to the 1998 Traffic Barricade Manual)

**-effective immediately-**

The City is requesting strict compliance/enforcement of the two new additions to the TBM, along with continuation of all other existing guidelines set forth. These two additions address situations that need more clarification in the TBM.

## ADD: Page 15, following 2<sup>nd</sup> paragraph:

Storing barricading in the public right-of-way is a privilege extended for the purpose of economy. In recent months, that privilege has been abused. A substantial amount of abuse relates to an increasing practice of contractors/city departments renting barricades from barricade companies under an agreement that the daily set-ups and take-downs will be done by the contractor. **Effective immediately**, it is required that each and every barricade rented be accompanied with the name and phone number of the responsible party, who will assure the take-downs and set-ups will be done in strict accordance with all provisions contained in the TBM. This is the same name and number that should already be placed on each Type I, II, and III barricades. Temporary traffic control devices are portable and need reasonable supervision to correct vandalism or displacement from weather, etc. No longer can temporary traffic control be stored on the right-of-way unattended for extended periods of time. Temporary traffic control must be checked at least once per day, including weekends, by the responsible party who must also be willing to provide 24-hour correction of deficiencies. If they are unwilling to do so, they must **alternatively include this service in their contract with the appropriate barricade company to do so for them.** The city's contact will be with the responsible party stenciled on the devices. If no response is forthcoming, the barricade company who owns the barricades will be called to correct the situation.

## ADD: Page 16, following 2<sup>nd</sup> paragraph:

**Specific placement and storage of barricade/sign devices on public right-of-way when not in active use.**

Property dedicated for public roadways belongs to the public at large, and the right-of-way needs to be kept clear for important public uses rather than for private benefit. Too often City staff encounters stored barricades/signs placed directly across sidewalks and at intersections, which virtually precludes access to buildings, facilities, walking areas and bus stops. When this happens, it either impedes accessibility to needed facilities, or requires the users to take lengthy alternate routes. This condition creates a tort liability risk for both the public entity and the company/individual responsible for using the right-of-way to store the devices.

To address this very need, the TBM is being modified to call for all stored barricade/traffic control devices to be **strategically** placed so as to "cluster" the devices in compact groups and placed in such a manner which does not cause additional obstructions to the public. Please refer to the appropriate accompanying photographs contained herein where "clustered" barricades are evident. By applying the "cluster" method of barricade storage, liability risks will diminish for all involved in construction and maintenance activities and enhance the aesthetic appearance of this great City.

ADD: Page 16, following above paragraph:

**Removal of advance warning signs and other associated temporary traffic control devices from public view when no longer in use and/or applicable.**

The TBM requires that advance warning signs and barricade devices be immediately removed from drivers' sight lines when no longer in use and/or applicable. Increasingly, our staff finds advance warning signs and associated temporary traffic control devices left in the street and sidewalks long after the construction and maintenance work activity has been completed or when no longer applicable. Please refer to the appropriate photographs contained herein where unacceptable barricade practices are evident.

It would be very much appreciated if you would promptly bring these situations to the attention of those employees, contractors, and inspectors who have the need to perform work or manage work in public streets for construction and maintenance purposes. The modifications described herein are effective immediately, and your full cooperation in compliance is required. Doing so will ensure that all users of the public right-of-way are provided with safe and accessible conditions, which is not only our collective goal, but also in the public interest.

Thank you for your immediate cooperation and assistance.



**These are acceptable conditions**

